## ACCESS POINT TYPES AND SPECIFICATIONS

USE	ROAD CLASSIFICATION	SECTION	STANDARD	STD. DWG	MINIMUM RADII (FT)	MINIMUM WIDTH (FT)	MAXIMUM WIDTH (FT)
RESIDENTIAL	I	CURB (URBAN)	DROP CURB DRIVEWAY	TABLE 2	N/A 2	TABLE 3	TABLE 3
		SHOULDER (RURAL)	RESIDENTIAL DRIVEWAY APPROACH:  -ASPHALT  -CONCRETE	SD 2-030 SD 2-035	10 10	TABLE 3 TABLE 3	TABLE 3 TABLE 3
COMMERCIAL OR	ARTERIAL	CURB (URBAN)	DROP CURB DRIVEWAY	TABLE 2	N/A 2	35	40
INDUSTRIAL		SHOULDER (RURAL)	COMMERCIAL/INDUSTRIAL APPROACH: -ASPHALT -CONCRETE	SD 2-040 SD 2-045	10 10	35 35	40 40
	NON-ARTERIAL	CURB (URBAN)	DROP CURB DRIVEWAY	TABLE 2	N/A 2	25	40
	ž	SHOULDER (RURAL)	COMMERCIAL/INDUSTRIAL APPROACH: -ASPHALT -CONCRETE	SD 2-040 SD 2-045	10 10	25 25	40 40

### TABLE 2

STD. DWG. NO.	DROP CURB DRIVEWAY TYPE
SD 2-020	TYPE 1 - PLANTER STRIP WITH RAMPED DRIVEWAY
SD 2-022	TYPE 2 - PLANTER STRIP WITH RAMPED SIDEWALK
SD 2-024	TYPE 3 — NO PLANTER STRIP, RAMPED SIDEWALK TYPE 3R — NO PLANTER STRIP, REVERSE SLOPE

#### NOTES:

TABLE 1 SPECIFICATIONS APPLY TO ALL ROAD NETWORK ELEMENTS EXCEPT PUBLIC ROADS.

2-010

- DROP CURB APPROACHES (TABLE 2) HAVE SIDE RAMPS IN LIEU OF RADII. SIDE RAMPS NOT PART OF A PEDESTRIAN ACCESS ROUTE SHALL NOT EXCEED 10% SLOPE MEASURED PARALLEL TO CURB LINE.
- 3. SEE EDDS 3-05 FOR ROAD NETWORK ELEMENT DESIGN DETAILS. SEE EDDS 2-03 FOR ONE-WAY ACCESS POINT WIDTHS AND URBAN CENTER ZONE SPECIFICATIONS.
- FIRE LANE STATUS MAY CHANGE MINIMUM WIDTH, SEE EDDS 2-03.A AND 3-01.C.



SNOHOMISH COUNTY PUBLIC WORKS

ACCESS POINT TYPES AND SPECIFICATIONS

APPROVED BY:

ROAD NETWORK

**ELEMENT** 

PRIVATE ROAD

SHARED COURT

SHARED DRIVEWAY

DRIVE AISLE

**ALLEY** 

DRIVEWAY

COUNTY ROAD ENGINEER

TABLE 3

MIN. WIDTH

(FT) (4)

20

20

16

20

10

10

MAX. WIDTH (FT)

36

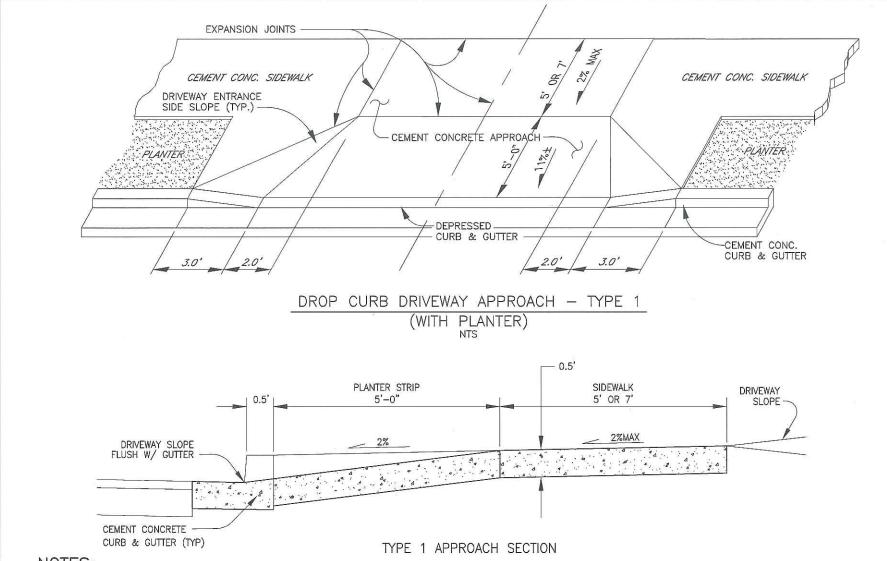
30

30

30

30

30



- 1. RECOMMEND SIDEWALK CROSS-SLOPES BE DESIGNED AT 1.5% OR LESS TO ENSURE ADA COMPLIANCE. SEE EDDS 4-05 FOR DETAILS
- 2. SEE STD DWG 2-028 FOR NOTES.

2 - 020



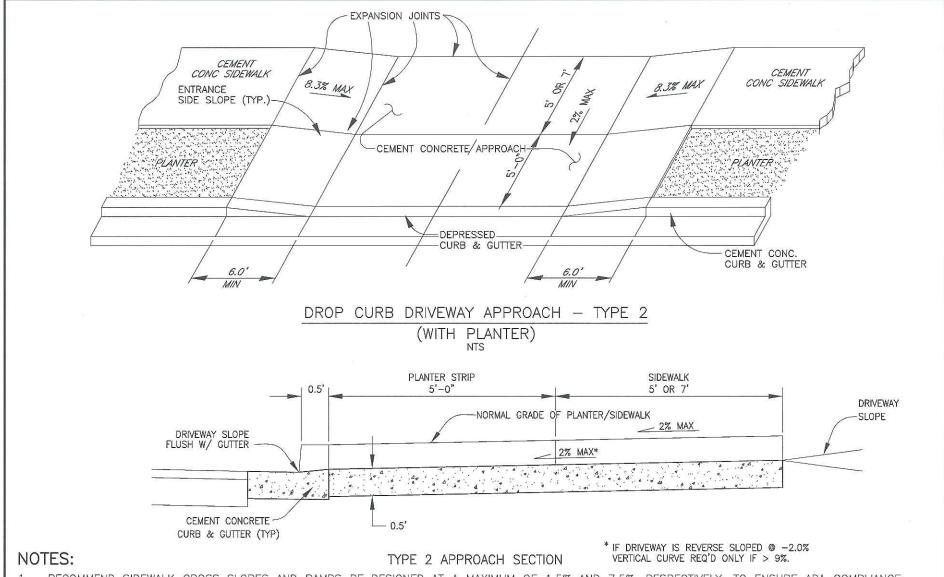
SNOHOMISH COUNTY PUBLIC WORKS

DROP CURB DRIVEWAY - TYPE 1

APPROVED BY:

COUNTY ROAD ENGINEER

/7//3 DATE



 RECOMMEND SIDEWALK CROSS SLOPES AND RAMPS BE DESIGNED AT A MAXIMUM OF 1.5% AND 7.5%, RESPECTIVELY, TO ENSURE ADA COMPLIANCE. SEE EDDS 4-05 FOR DETAILS.

2. SEE STD DWG 2-028 FOR NOTES

2 - 022

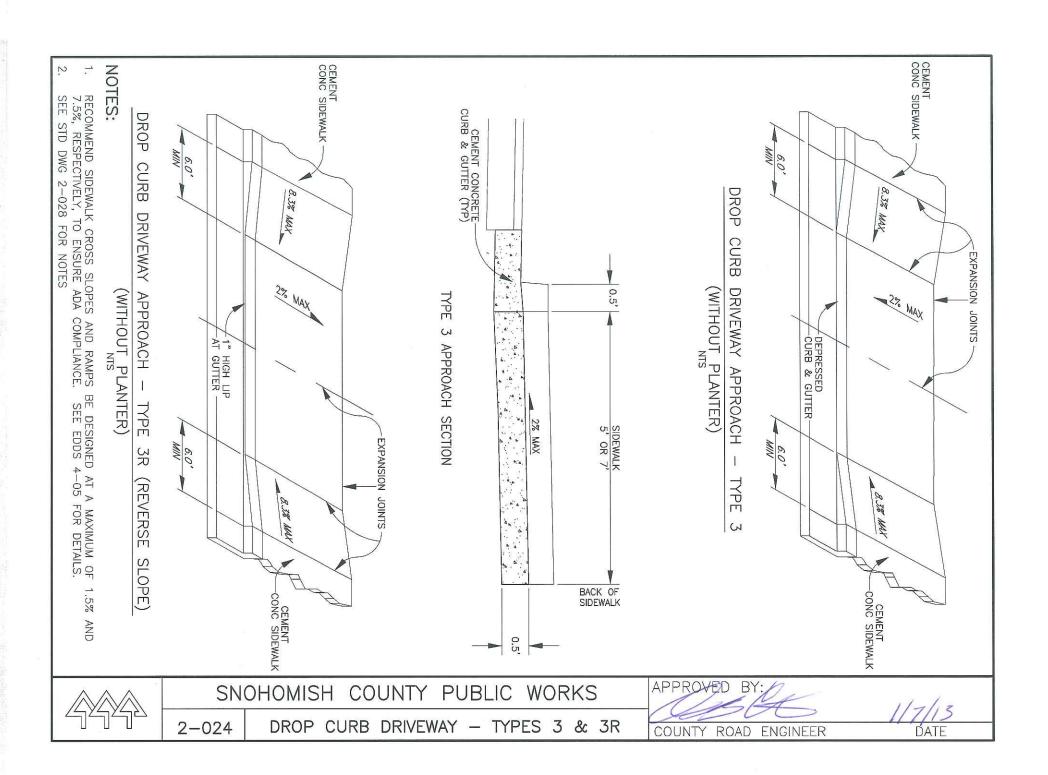


SNOHOMISH COUNTY PUBLIC WORKS

DROP CURB DRIVEWAY - TYPE 2

APPROVED BY:

COUNTY ROAD ENGINEER



- SEE STD DWG 2-010 TO DETERMINE THE APPLICABLE ACCESS POINT DESIGN AND SPECIFICATIONS.
- SEE EDDS 4-04 AND WSDOT STD PLANS F-10.12 AND F-10.16 FOR CURB DETAILS.
- 3. SEE EDDS 4-05 AND STD DWG 4-150 FOR SIDEWALK SPECIFICATIONS. NOTE DESIGN GUIDANCE TO ENSURE ADA COMPLIANCE IN CONSTRUCTION.
- 4. ALL COMMERCIAL OR INDUSTRIAL ACCESS POINTS, INCLUDING THE CURB, GUTTER AND SIDEWALK, SHALL MEET AASHTO HL-93 LOADING REQUIREMENTS.
- 5. DRIVEWAY CEMENT CONCRETE DEPTH SHALL BE A MINIMUM OF 6 INCHES AND PLACED ON COMPACTED GRADE.
- THE DROP CURB DRIVEWAY SECTION BETWEEN A SIDEWALK AND THE CURB AND GUTTER SHALL BE CONCRETE.
- 7. CONCRETE SHALL BE AIR-ENTRAINED CONCRETE CLASS 4000 PER WSDOT STD SPECIFICATION 8-06.3.
- 8. ALL JOINTS SHALL BE CLEANED AND EDGED.

2 - 028

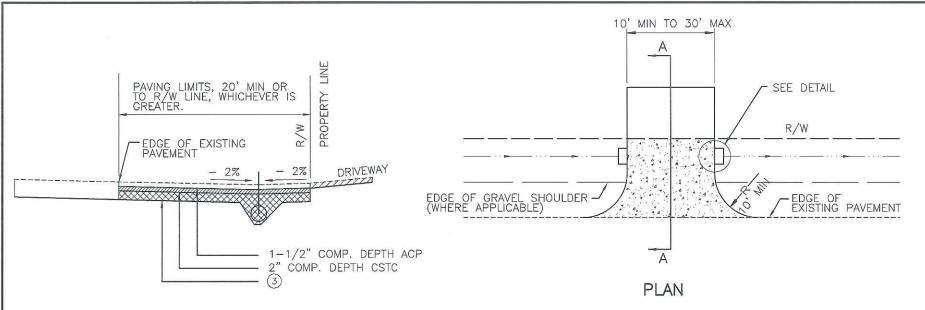
- 9. FOR A DRIVEWAY WIDTH EXCEEDING 15 FEET, A FULL DEPTH EXPANSION JOINT WITH 3/8-INCH JOINT FILLER IS REQUIRED ALONG THE DRIVEWAY CENTERLINE. PARALLEL EXPANSION JOINTS ARE REQUIRED AT 15 FT MAXIMUM SPACING FOR DRIVEWAY WIDTHS EXCEEDING 30 FEET. REFER TO WSDOT STANDARD PLAN F-30.10 AND STANDARD SPECIFICATION 5-05.3(8).
- 10. STANDARD 3/8-INCH EXPANSION JOINTS SHALL BE PLACED AT BACK, FRONT AND SIDES OF DRIVEWAY APPROACHES AS SHOWN ON STD DWGS.
- 11. AVOID PLACING DRAINAGE STRUCTURES, JUNCTION BOXES OR OTHER OBSTRUCTIONS IN FRONT OF DRIVEWAY ENTRANCES.

SNOHOMISH COUNTY PUBLIC WORKS

DROP CURB DRIVEWAY NOTES

APPROVED BY:

COUNTY ROAD ENGINEER



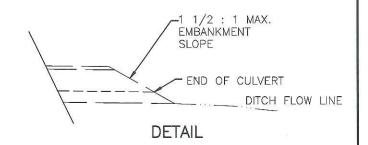
### SECTION A-A

#### NOTES:

- FOR DRIVEWAY OR SHARED DRIVEWAY ACCESS IN A RURAL SECTION. FIRE LANE SPECIFICATIONS MAY APPLY, SEE EDDS 2-03.A AND 3-01.C.
- ALL SURFACE DRAINAGE FROM THE DRIVEWAY MUST BE CONTAINED AND DIRECTED FROM THE DRIVEWAY TO THE OPEN DITCH. NO SURFACE DRAINAGE SHALL FLOW ONTO THE COUNTY ROAD.
- 3. SUBGRADE SHALL BE PLACED AND COMPACTED IN ACCORDANCE WITH SECTION 2-03.3(14)C OF THE WSDOT/APWA SPECIFICATIONS (METHOD B). SURFACING MATERIALS SHALL BE COMPACTED TO 95% OF MAXIMUM DENSITY (MODIFIED PROCTOR).
- 4. CULVERT PIPE SHALL BE SIZED TO ACCOMMODATE DITCH FLOWS BUT IN NO CASE BE SMALLER THAN 12 INCHES. BEVEL CULVERT ENDS TO MATCH SIDESLOPES.
- 5. COVER DEPTHS LESS THAN 12 INCHES REQUIRE APPROVAL BY THE ENGINEER.
- 6. A DRIVEWAY CULVERT HEADWALL, SUBJECT TO APPROVAL BY THE ENGINEER, MAY BE USED IN LIEU OF THE 1 1/2: 1 MAX. SIDESLOPE.

SEE TEXT SECTION 2-03 & 5-05.L

2 - 030





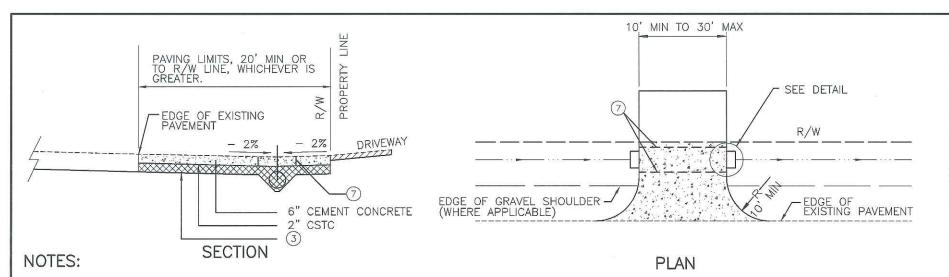
# SNOHOMISH COUNTY PUBLIC WORKS

RESIDENTIAL DRIVEWAY APPROACH - ASPHALT

APPROVED BY

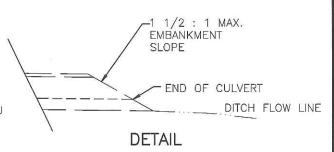
COUNTY ROAD ENGINEER

1/7/13



- FOR DRIVEWAY OR SHARED DRIVEWAY ACCESS IN A RURAL SECTION. FIRE LANE SPECIFICATIONS MAY APPLY, SEE EDDS 2-03.A AND 3-01.C.
- ALL SURFACE DRAINAGE FROM THE DRIVEWAY MUST BE CONTAINED AND DIRECTED FROM THE DRIVEWAY TO THE OPEN DITCH. NO SURFACE DRAINAGE SHALL FLOW ONTO THE COUNTY ROAD.
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- 6. A DRIVEWAY CULVERT HEADWALL, SUBJECT TO APPROVAL BY THE ENGINEER, MAY BE USED IN LIEU OF THE 1 1/2: 1 MAX. SIDESLOPE.
- 7. EXPANSION JOINT REQUIRED AT A DISTANCE OF 1 1/2 TIMES THE PIPE DIAMETER FROM PIPE CENTERLINE. MATERIAL MAY BE CEDAR 2"X6" OR 3/8" MIN. X FULL DEPTH PREMOLDED JOINT MATERIAL.
- 8. PAVEMENT/DRIVEWAY INTERFACE MUST BE A CLEAN STRAIGHT SURFACE WITH A 3/8" MIN. X FULL DEPTH EXPANSION JOINT. 3/8" MIN. THICKNESS OF PREMOLDED JOINT MATERIAL REQUIRED. (NO CEDAR).

SEE TEXT SECTION 2-03 & 5.05.L





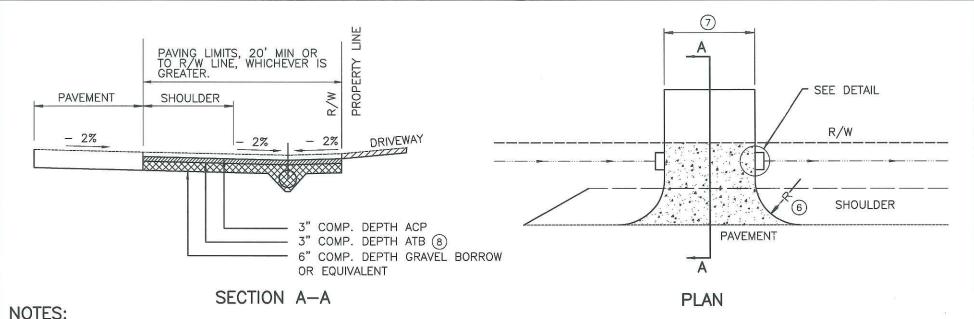
SNOHOMISH COUNTY PUBLIC WORKS

2-035 RESIDENTIAL DRIVEWAY APPROACH - CONCRETE

APPROVED BY:

1/7/13 DATE

COUNTY ROAD ENGINEER DA



- 1. ALL SURFACE DRAINAGE FROM THE DRIVEWAY MUST BE CONTAINED AND DIRECTED FROM THE DRIVEWAY TO THE OPEN DITCH. NO SURFACE DRAINAGE SHALL FLOW ONTO THE COUNTY ROAD.
- 2. SUBGRADE SHALL BE PLACED AND COMPACTED IN ACCORDANCE WITH SECTION 2-03.3(14)C OF THE WSDOT/APWA SPECIFICATIONS (METHOD B). SURFACING MATERIALS SHALL BE COMPACTED TO 95% MAXIMUM DENSITY (MODIFIED PROCTOR).
- 3. CULVERT PIPE SHALL BE 12 INCHES MINIMUM DIAMETER AND LARGER IF DRAINAGE REQUIRES.
- 4. COVER DEPTHS LESS THAN 12" REQUIRE REINFORCED CONCRETE CULVERT PIPE AND APPROVAL BY THE ENGINEER.
- 5. A DRIVEWAY CULVERT HEADWALL, SUBJECT TO APPROVAL BY THE ENGINEER, MAY BE USED IN LIEU OF THE 1 1/2:1 SIDESLOPE.
- 6. SEE STANDARD DRAWING 2-010 FOR RADII.

2 - 040

- MAXIMUM WIDTH: 40'. MINIMUM WIDTH SEE SECTION 2-03.
- 8. ADDITIONAL PAVEMENT THICKNESS MAY BE REQUIRED FOR HEAVY TRUCK TRAFFIC.

SEE TEXT SECTION 2-03

END OF CULVERT DITCH FLOW LINE DETAIL

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SNOHOMISH COUNTY PUBLIC WORKS

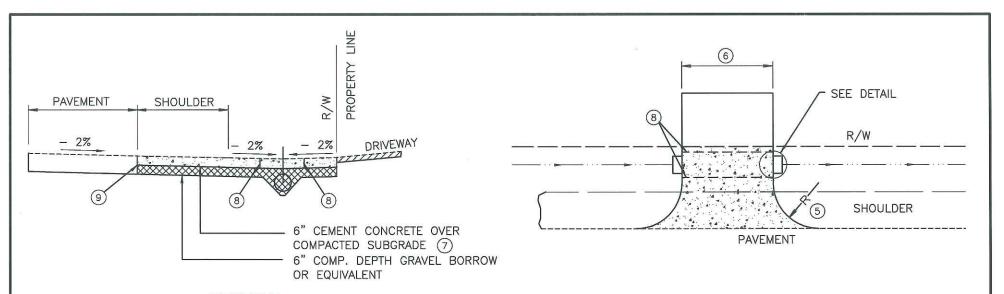
COMMERCIAL/INDUSTRIAL APPROACH—ASPHALT

APPROVED BY:

 $1 \ 1/2 : 1$ 

MAX. SLOPE -

Connew 2-25-03 COUNTY ROAD ENGINEER DATE

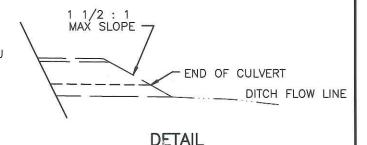


SECTION

- 1. ALL SURFACE DRAINAGE FROM THE DRIVEWAY MUST BE CONTAINED AND DIRECTED FROM THE DRIVEWAY TO THE OPEN DITCH. NO SURFACE DRAINAGE SHALL FLOW ONTO THE COUNTY ROAD.
- 2. CULVERT PIPE SHALL BE 12 INCHES MINIMUM DIAMETER AND LARGER IF DRAINAGE REQUIRES.
- 3. COVER DEPTHS LESS THAN 12 INCHES REQUIRE REINFORCED CONCRETE CULVERT PIPE AND APPROVAL BY THE ENGINEER.
- 4. A DRIVEWAY CULVERT HEADWALL, SUBJECT TO APPROVAL BY THE ENGINEER, MAY BE USED IN LIEU OF THE  $1\ 1/2\ : 1$  SIDESLOPE.
- 5. SEE STANDARD DRAWING 2-010 FOR RADII.
- 6. MAXIMUM ACCESS POINT WIDTH: 40'. MINIMUM WIDTH SEE SECTION 2-03.
- 7. ADDITIONAL PAVEMENT THICKNESS MAY BE REQUIRED FOR HEAVY TRUCK TRAFFIC.
- 8. EXPANSION JOINT REQUIRED AT A DISTANCE OF 1 1/2 TIMES THE PIPE DIAMETER FROM PIPE CENTERLINE. MATERIAL MAY BE CEDAR 2"X6" OR 3/8" MIN. X FULL DEPTH PREMOLDED JOINT MATERIAL.
- 9. PAVEMENT/DRIVEWAY INTERFACE MUST BE A CLEAN STRAIGHT SURFACE WITH A 3/8" MIN. X FULL DEPTH EXPANSION JOINT. 3/8" MIN THICKNESS OF PREMOLDED JOINT MATERIAL REQUIRED. (NO CEDAR).

SEE TEXT SECTION 2-03

PLAN



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SNOHOMISH COUNTY PUBLIC WORKS

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Thomsen

2-25-03

2-045 COMMERCIAL/INDUSTRIAL APPROACH - CONCRETE

COUNTY ROAD ENGINEER

APPROVED BY:

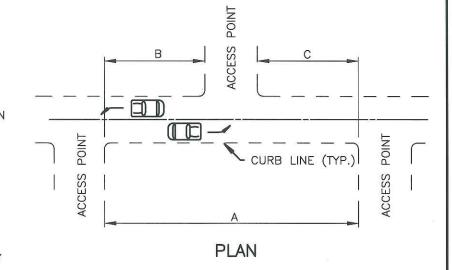
# MINIMUM ACCESS POINT SPACING - COMMERCIAL/INDUSTRIAL (FEET) 1 6

ROADWAY SPEED		MENSION A	DIM	ENSION B (5)	DIMENSION C (5)	
(MPH) (2)	ARTERIALS (3)	NON-ARTERIALS (4)	ARTERIALS	NON-ARTERIALS	ARTERIALS	NON-ARTERIALS
25	105	35	105	75	105	35
30	125	40	125	75	125	40
35	150	45	150	75	150	45
40	185	50	185	75	185	50
45	230	50	230	75	230	50
50	275	50	275	75	275	50

### NOTES:

- 1. ACCESS POINT SPACING ONLY. FOR PUBLIC STREET SPACING, SEE TEXT SECTION 3-09.
- 2. REFERS TO POSTED SPEED OR OPERATING SPEED, WHICHEVER IS GREATER.
- BETWEEN THE NEAREST EDGES OF TWO-WAY ACCESS POINTS. DISTANCES BETWEEN ADJACENT, ONE-WAY ACCESS POINTS (WITH THE INBOUND ACCESS UPSTREAM) CAN BE ONE-HALF THE DISTANCES SHOWN ABOVE.
- 4. BETWEEN THE NEAREST EDGES OF ONE OR TWO-WAY ACCESS POINTS.
- 5. ACCESS POINTS DIRECTLY OPPOSITE FROM EACH OTHER ARE MOST DESIRABLE. WHERE THIS IS NOT POSSIBLE, THESE DIMENSIONS WILL APPLY.
- 6. WHERE ACCESS POINTS ARE TO BE SIGNALIZED. A MINIMUM SPACING OF 1200 FEET TO ANY OTHER SIGNALIZED INTERSECTION SHOULD BE MAINTAINED. IF THE SIGNALIZED ACCESS POINTS FORM A "T" INTERSECTION WITH LITTLE POSSIBILITY OF ANY FUTURE ACCESS POINT ACROSS THE STREET, A MINIMUM SPACING OF 600 FEET FROM THE NEAREST SIGNALIZED INTERSECTION MAY BE ACCEPTABLE.
- 7. IN CASES WHERE ACCESS POINT SPACING IS NOT ATTAINABLE BECAUSE EXISTING FRONTAGES ARE NARROW, ACCESS POINTS SHOULD BE LOCATED AS CLOSE TO THE TABULATED VALUES SHOWN ABOVE AS POSSIBLE. WHEN THIS OCCURS, THE ENGINEER MAY REQUIRE ANALYSIS TO DETERMINE IF LEFT TURNS SHOULD BE PROHIBITED INTO OR OUT OF THE ACCESS POINT.

SEE TEXT SECTION 2-04



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APPROVED BY:

2-7-03

2-050 COMMERCIAL/INDUSTRIAL ACCESS POINT SPACING

COUNTY ROAD ENGINEER

DATE

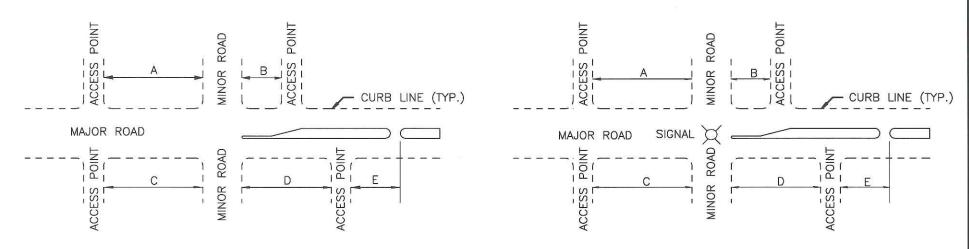
SNOHOMISH COUNTY PUBLIC WORKS

## MINIMUM CORNER CLEARANCES FOR STOP SIGN INTERSECTION CONTROL (IN FEET)

# MINIMUM CORNER CLEARANCES FOR SIGNALIZED INTERSECTION CONTROL (IN FEET)

			NON-ARTERIALS					
		ARTERIAL POSTED SPEED						
DIM.	30	35	40	45	50	55	ALL SPEEDS	
Α	115	135	160	180	205	230	50	
В	85	105	120	140	155	170	50	
С	115	135	160	180	205	230	50	
D	115	135	160	180	205	230	50	
Ε	115 OR 0	135 OR 0	160 OR 0	180 OR 0	205 OR 0	230 OR 0	0	

		NON-ARTERIALS					
		- ALL SPEEDS					
DIM.	30	35	40	45	50	55	ALL SI LLDS
Α	230	275	320	365	410	460	50
В	115	135	160	180	205	230	50
С	230	275	320	365	410	460	50
D	230	275	320	365	410	460	50
Е	115 OR 0	135 OR 0	160 OR 0	180 OR 0	205 OR 0	230 OR 0	0



## NOTES:

- 1. IN CASES WHERE CORNER CLEARANCES ARE NOT ATTAINABLE BECAUSE FRONTAGES ARE NARROW, ACCESS POINTS SHOULD BE LOCATED AS CLOSE AS PRACTICABLE TO THE PROPERTY LINE MOST DISTANT FROM THE INTERSECTION. THE ENGINEER MAY REQUIRE ANALYSIS OF SUCH LOCATIONS TO DETERMINE IF LEFT TURNS SHOULD BE PROHIBITED INTO OR OUT OF THE ACCESS POINT.
- 2. ACCESS POINTS NEAR STOP OR SIGNAL CONTROLLED INTERSECTIONS SHALL BE ANALYZED TO DETERMINE WHETHER STOPPING QUEUES WILL BLOCK THE ACCESS POINT.

SEE TEXT SECTION 2-05

2 - 060

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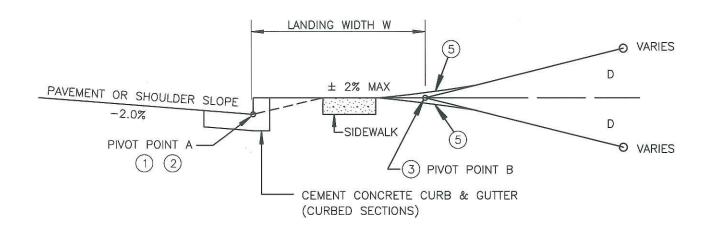
SNOHOMISH COUNTY PUBLIC WORKS

APPROVED BY:

WEllermen 2-7-03

COMMERCIAL/INDUSTRIAL CORNER CLEARANCES

S COUNTY ROAD ENGINEER



- 1. SEE STD DWG 4-140 FOR CURB DETAILS.
- 2. WHEN ACCESSING SHOULDERED ROADWAYS, MAINTAIN SHOULDER SLOPE TO PIVOT POINT A.
- 3. ACCESS POINT GRADE SHALL BE MEASURED FROM PIVOT POINT B.
- 4. LANDING WIDTH W MAY BE REDUCED SUBJECT TO APPROVAL OF THE ENGINEER IN ACCORDANCE WITH SECTION 1-05 OF THESE STANDARDS.
- 5. A VERTICAL CURVE SHALL BE CONSTRUCTED TO TRANSITION THE LANDING TO THE ACCESS APPROACH. THE VERTICAL SEPARATION BETWEEN THE CURVE AND A 10-FOOT CHORD OF THE CURVE SHALL NOT EXCEED 3.25 INCHES (WHERE D IS POSITIVE) OR 2.00 INCHES (WHERE D IS NEGATIVE).
- 6. GRADE ACROSS RURAL LANDING MAY BE ±5%.

SEE TEXT SECTION 2-07

TYPE OF ACCESS ACCESSING LANDING WIDTH W 4 ACCESS GRADE D

TITE OF MODESS	71002331110	BUILDING MIDITI II	7100200	OTTOL D
RESIDENTIAL (URBAN)	NON-ARTERIAL	15'	±	15% MAX.
RESIDENTIAL (URBAN)	ARTERIAL	15'	±	7% MAX.
RESIDENTIAL (RURAL)	ALL	5' ⑥	±	15% MAX.
COMMERCIAL/INDUSTRIAL	NON-ARTERIAL	30'	±	8% MAX.
COMMERCIAL/INDUSTRIAL	ARTERIAL	30'	±	5% MAX.

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SNOHOMISH COUNTY PUBLIC WORKS

APPROVED BY:

Morrison 2-25-03

2 - 070ACCESS POINT GRADES

COUNTY ROAD ENGINEER